

FED. ROAD DIST. NO.	STATE	JOB/PIECE NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.				
DESCRIPTION		REVISIONS	DATE		

TRAFFIC DESIGN

PROJECT ENGINEER: A. ENGINEER  
SQUAD SUPERVISOR: A. TECHNICIAN

ROADWAY DESIGN

PROJECT ENGINEER: A. ENGINEER  
SQUAD SUPERVISOR: A. TECHNICIAN

BRIDGE DESIGN

SQUAD SUPERVISOR: D. HARJO  
SQUAD MEMBERS: V. TRAN, G. BAPTISTE,  
W. YANES, H. SKILLINGS

ENGINEERING MANAGER: MOLLA-ESMAIL, P.E.

STATE OF OKLAHOMA  
DEPARTMENT OF TRANSPORTATION

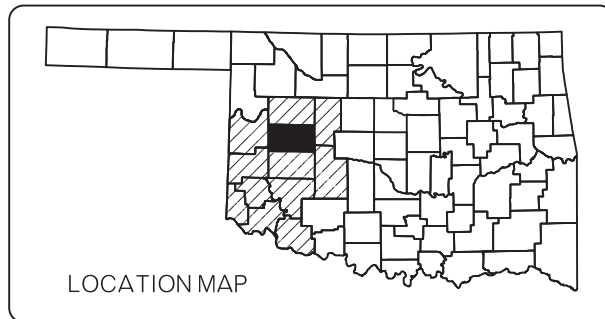
PLAN OF PROPOSED  
STATE HIGHWAY

FEDERAL AID PROJECT NO. NHPPI-NBIP-(505)3B  
BRIDGE PREVENTATIVE MAINTENANCE (PAINT) - DIVISION 5  
STATE HIGHWAY I-40

CUSTER COUNTY

CONTROL SECTION NO.: 40-20-02  
STATE JOB NO. 31700(04)

BRIDGE "A" LOCATION NO. 2002 1355NX; EXISTING NBIS NO. 17528  
BRIDGE "B" LOCATION NO. 2002 1355SX; EXISTING NBIS NO. 17512



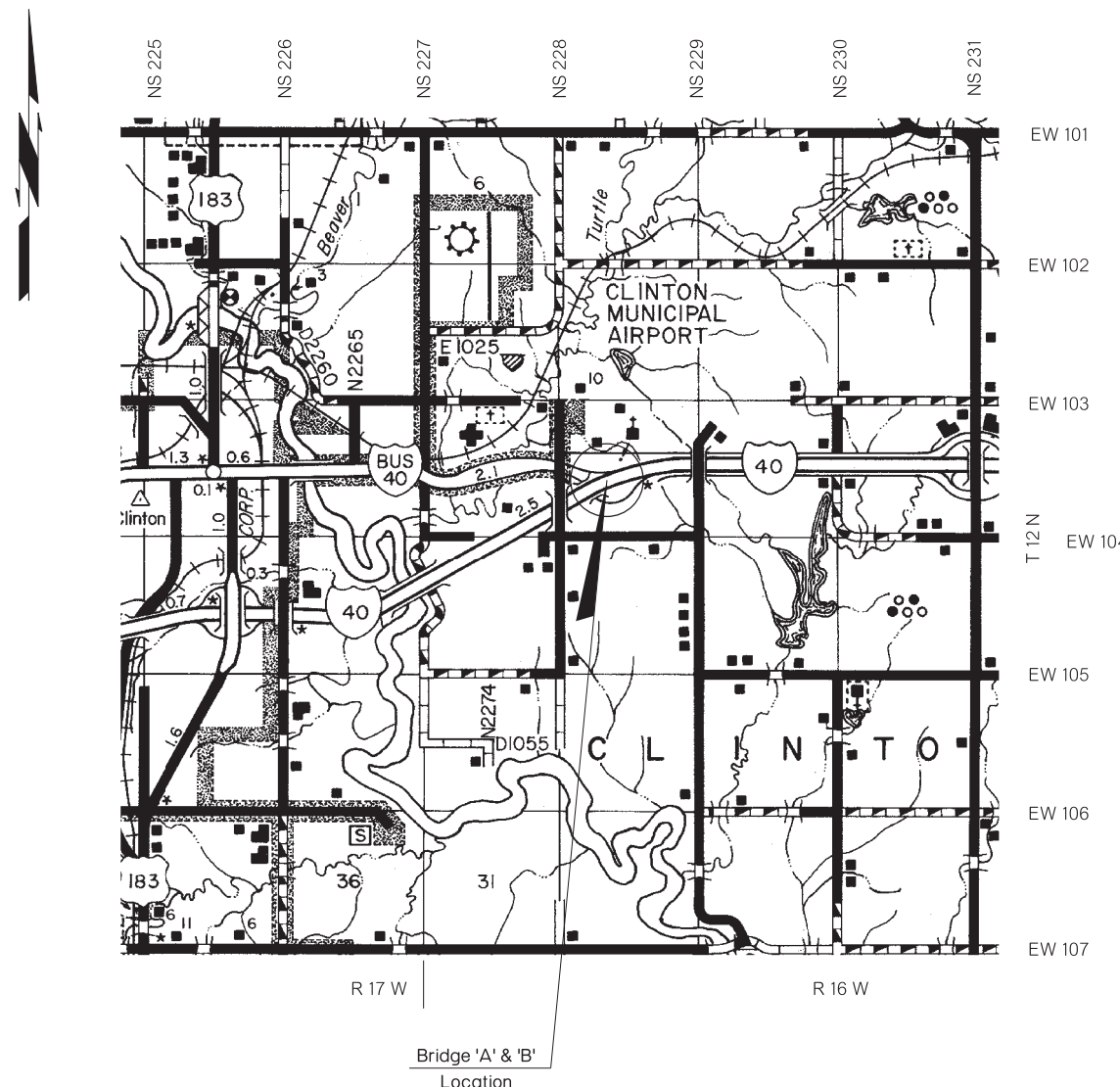
INDEX OF SHEETS

SHEET NUMBER	SHEET DESCRIPTION
0001	TITLE SHEET
AB01	GENERAL NOTES AND SUMMARY OF PAY QUANTITIES (BRIDGE)
AT01	SUMMARY OF PAY QUANTITIES AND NOTES (TRAFFIC CONTROL)
B001	GENERAL PLAN AND ELEVATION (BRIDGE "A")
B002	GENERAL PLAN AND ELEVATION (BRIDGE "B")
T001	TRAFFIC CONTROL DETAIL I-40 BRIDGE OVER GARY BLVD.
T002	I-40 BRIDGE OVER GARY BLVD.

STANDARDS TO BE INCLUDED

TRAFFIC

- TCS1-1-01
- TCS2-1-00
- TCS3-1-01
- TCS4-1-01
- TCS5-1-00
- TCS6-1-02
- TCS7-1-02
- TCS8-1-00
- TCS9-1-01
- TCS10-1-00
- TCS11-1-01
- TCS14-1-00
- TCS18-1-01
- TCS19-1-01
- TCS20-1-00
- TCS21-1-02





CONVENTIONAL SYMBOLS

- PROPOSED ROAD
- RAILROADS
- RANGE & TOWNSHIP
- SECTION LINES
- QUARTER SECTION LINES
- FENCES
- GROUND LINE
- EXISTING ROADS
- BASE LINE
- GRADE LINES
- TELEPHONE & TELEGRAPH
- POWER LINES
- BUILDINGS
- OILWELL
- DRAINAGE STRUCTURES - IN PLACE
- DRAINAGE STRUCTURES - NEW
- RIGHT-OF-WAY LINES - EXISTING
- RIGHT-OF-WAY LINES - NEW
- CONTROLLED ACCESS
- RIGHT-OF-WAY FENCE

2009 OKLAHOMA STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION GOVERN, APPROVED BY THE U.S. DEPARTMENT OF TRANSPORTATION, FEDERAL HIGHWAY ADMINISTRATION, JANUARY 4, 2010.

PREPARED BY:  
OKLAHOMA DEPARTMENT OF TRANSPORTATION  
BRIDGE DESIGN DIVISION  
ASGHAR MOLLA-ESMAIL, P.E.  
OKLA. REG. NO. 17544  
DATE 6/15/2016

OKLAHOMA DEPARTMENT OF TRANSPORTATION	DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION
DATE APPROVED _____	DATE APPROVED _____
BY _____ CHIEF ENGINEER	BY _____ DIVISION ADMINISTRATOR
SWO _____	PROJECT NO. NHPPI-NBIP-(505)3B
COUNTY CUSTER	HIGHWAY IS 40 SHEET NO. 0001

REVISIONS		
REV. NO.	DESCRIPTION	DATE

GENERAL NOTES

**SPECIFICATIONS:**

COMPLY WITH THE REQUIREMENTS OF THE 2009 STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EXCEPT AS MODIFIED BY THE PLANS AND SPECIAL PROVISIONS.

**VERIFICATION OF EXISTING CONDITIONS:**

BIDDERS SHALL FULLY INFORM THEMSELVES OF THE NATURE OF THE WORK AND CONDITIONS UNDER WHICH IT WILL BE PERFORMED. THE CONTRACTOR SHALL ADOPT METHODS CONSISTENT WITH GOOD CONSTRUCTION PRACTICE AND SHALL TAKE ALL NECESSARY PRECAUTIONS TO PREVENT DAMAGE TO ANY EXISTING BRIDGE STRUCTURE OR ROADWAY. ANY DAMAGE TO THE BRIDGE STRUCTURE OR ROADWAY DUE TO THE CONTRACTOR'S NEGLIGENCE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE, TO THE SATISFACTION OF THE ENGINEER.

**PLANS:**

CONSTRUCTION PLANS FOR THE EXISTING STRUCTURES, MAY BE OBTAINED FROM THE REPRODUCTIONS BRANCH OF THE OKLAHOMA DEPARTMENT OF TRANSPORTATION.

REPRODUCTION BRANCH  
OKLAHOMA DEPARTMENT OF TRANSPORTATION  
200 NE 21ST STREET  
OKLAHOMA CITY, OKLAHOMA 73105

ASK FOR:  
FEDERAL AID PROJ. NO. I-40-2(60)65 FOR BRIDGES 'A' AND 'B'.

**DESCRIPTION OF WORK:**

THE PROJECT CONSISTS OF PAINTING THE EXISTING STEEL ON THE BRIDGE SUPERSTRUCTURE. CARE SHALL BE TAKEN TO ENSURE THAT NO DAMAGE IS CAUSED TO THE TRAFFIC DURING CLEANING AND PAINTING. TARPS AND OTHER NECESSARY EQUIPMENT WILL BE USED AS APPROVED BY THE ENGINEER.

**SSPC QP-2 CERTIFICATION:**

SSPC QP-2 CERTIFICATION WILL BE REQUIRED FOR THIS PROJECT.  
PROVIDE QP-2 CERTIFICATION TO THE RESIDENT ENGINEER PRIOR TO START OF WORK.

**CLEANING OF DEBRIS:**

THE CONTRACTOR SHALL REMOVE ALL DEBRIS FROM THE TOPS OF PIERS AND BRIDGE SEATS. ALL COSTS TO CLEAN THE DEBRIS SHALL BE INCLUDED IN OTHER ITEMS OF WORK.

**EXPOSURE OF DETERIORATED STRUCTURAL STEEL:**

IF ANY DETERIORATED STRUCTURAL STEEL IS EXPOSED DURING CLEANING THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING THE RESIDENT ENGINEER WHO IN TURN WILL NOTIFY THE BRIDGE ENGINEER AS TO THE EXTENT OF THE DAMAGE. THE BRIDGE ENGINEER SHALL DETERMINE IF ANY REPAIRS ARE NECESSARY AND IF SO, WHAT METHOD OF REPAIR SHALL BE USED.

**LABEL PAINT SYSTEM:**

THE CONTRACTOR SHALL STENCIL THE PAINT TYPES, MANUFACTURER'S NAME, AND DATE OF COMPLETION INSIDE THE EXTERIOR GIRDER ON THE SOUTHWEST CORNER OF BRIDGE IN ACCORDANCE WITH SECTION 512 OF THE STANDARD SPECIFICATIONS.

**WORK SITE OBSTRUCTIONS:**

ALL OBSTRUCTIONS INCLUDING VEGETATION AND DEBRIS, INTERFERING WITH PAINT OPERATIONS BEING PERFORMED SHALL BE REMOVED BY THE CONTRACTOR PRIOR TO BEGINNING WORK. ALL COSTS ASSOCIATED WITH REMOVAL OF OBSTRUCTIONS SHALL BE INCLUDED IN OTHER ITEMS OF WORK.

PAY ITEM NOTES

- (1) PAY ITEM "PAINTING EXISTING STRUCTURES" SHALL CONSIST OF CLEANING AND PAINTING OF ALL BRIDGE SUPERSTRUCTURE SECTIONS INCLUDING BRACINGS, BEARING DEVICES AND ANY OTHER MISCELLANEOUS ITEMS AS SPECIFIED IN SECTION 512 OF THE STANDARD SPECIFICATIONS FOR A CATEGORY "E" APPLICATION. PAINT METAL HANDRAILS AND HANDRAIL POSTS ON THE BRIDGES AS SPECIFIED IN THE STANDARD SPECIFICATIONS FOR A CATEGORY "O" APPLICATION.  
THE PAINT SYSTEM AND APPLICATION THEREOF SHALL BE IN ACCORDANCE WITH SECTION 730 "PAINT FOR STRUCTURAL STEEL" OF THE STANDARD SPECIFICATIONS.  
ALL COSTS INCLUDING CLEANING, PAINTING, EQUIPMENT, LABOR AND INCIDENTALS WILL BE INCLUDED IN PRICE BID PER LUMP SUM OF "PAINTING EXISTING STRUCTURES"
- (2) PAY ITEM "COLLECTION AND HANDLING OF WASTE" SHALL INCLUDE ALL LABOR, MATERIALS AND INCIDENTALS NECESSARY FOR CLEANING, CONTAINMENT, STABILIZATION, INCINERATION, TRANSPORTATION AND DISPOSAL OF WASTE, SAMPLING AND TESTING OF SOIL, OR AND WASTE MATERIALS, PERMITS AND RELATED ITEMS OTHER THAN PAINTING AS SPECIFIED IN THE STANDARD SPECIFICATIONS.  
PAINT ON THESE STRUCTURE CONTAIN LEAD. THE CONTRACTOR IS RESPONSIBLE FOR TESTING AND WILL COMPLY WITH SECTION 512 OF THE STANDARD SPECIFICATIONS FOR HAZARDOUS AND NON-HAZARDOUS WASTE.  
THE ON SITE LOCATION FOR TEMPORARY STORAGE OF LEAD WASTE MUST BE APPROVED BY THE ENGINEER.
- (3) STR. 'A' ESTIMATED AREA OF STRUCTURAL STEEL TO BE PAINTED IS APPROXIMATELY 12,000 SQUARE FEET. STR. 'B' ESTIMATED AREA OF STRUCTURAL STEEL TO BE PAINTED IS APPROXIMATELY 10,500 SQUARE FEET. THE AREA SHOWN FOR STRUCTURAL STEEL IS APPROXIMATE, AND THE STATE ASSUMES NO RESPONSIBILITY FOR THE ACCURACY THEREOF. NO COMPENSATION WILL BE ALLOWED FOR ERRORS IN THESE ESTIMATES.

31700(04) PAY QUANTITIES				
0200 BRIDGE 'A' - NBI 17528				
ITEM		DESCRIPTION	UNIT	QUANTITY
512(A)	1323	PAINTING EXISTING STRUCTURES (1) (3)	LSUM	1.00
512(B)	6303	COLLECTION AND HANDLING OF WASTE (2) (3)	LSUM	1.00

31700(04) PAY QUANTITIES				
0201 BRIDGE 'B' - NBI 17512				
ITEM		DESCRIPTION	UNIT	QUANTITY
512(A)	1323	PAINTING EXISTING STRUCTURES (1) (3)	LSUM	1.00
512(B)	6303	COLLECTION AND HANDLING OF WASTE (2) (3)	LSUM	1.00

31700(04) PAY QUANTITIES				
0640 CONSTRUCTION				
ITEM		DESCRIPTION	UNIT	QUANTITY
641	1399	MOBILIZATION	LSUM	1.00

ENVIRONMENTAL MITIGATION NOTES

CLIFF SWALLOWS AND BARN SWALLOWS ARE SMALL COLONIAL NESTING BIRDS PROTECTED BY THE FEDERAL MIGRATORY BIRD TREATY ACT. THESE SPECIES COMMONLY USE BRIDGES AND CULVERTS FOR NESTING. THE NESTING SEASON FOR THE SWALLOWS RUNS FROM APRIL 1 TO AUGUST 31. ANY ACTIVITIES WHICH WOULD DESTROY ACTIVE NESTS OR HARM EGGS OR BIRDS WOULD VIOLATE THE MIGRATORY BIRD TREATY ACT. SWALLOW USE OF BRIDGE NBI NO. 17528 & 17512 WAS NOT OBSERVED DURING THE INITIAL SURVEYS CONDUCTED AS PART OF THE BIOLOGICAL STUDIES IN 2013. SWALLOWS MAY OCCUPY THE BRIDGE IN THE FUTURE NESTING SEASONS. THE RESIDENT ENGINEER WILL EVALUATE THE CONTRACTOR'S PROPOSED WORK METHODS AND CONCLUDE WHETHER THE PROPOSED WORK WOULD POSE DISRUPTION TO ANY NESTING BIRDS BEFORE WORK NEAR THE STRUCTURE IS AUTHORIZED. IF THE PROPOSED WORK WILL HARM ANY NESTING BIRDS, THE BRIDGE MAY BE NETTED PRIOR TO APRIL 1 OR THE WORK DELAYED UNTIL THE NESTING SEASON IS COMPLETE. METHODS OTHER THAN NETTING MUST BE PRE-APPROVED BY THE ODOT BIOLOGIST.

LOCATIONS OUTSIDE THE PROJECT AREA IN THE FOLLOWING AREA MUST NOT BE UTILIZED FOR BORROW, EQUIPMENT STAGING, HAUL ROADS, SPOIL DUMPS OR ANY OFF-SITE PROJECT-RELATED ACTIVITY.

T12N R16W:

SECTION 18: SE1/4 S1/2 SE1/4 NE1/4 SE1/4 SW1/4

SECTION 19: N1/2 NW1/4 N1/2 NW1/4 NE1/4

<p>CUSTER COUNTY</p> <p>GENERAL NOTES AND SUMMARY OF PAY QUANTITIES (BRIDGE)</p> <p>Squad: HARJO Engr: MOLLA-ESMAIL</p>			Design	N/A	N/A
			Detail	DAH	04/16
			Check		
STATE OF OKLAHOMA	DEPARTMENT OF TRANSPORTATION		JOB/PIECE NO. 31700(04)	SHEET NO. AB01	

TRAFFIC OPERATIONS GENERAL CONSTRUCTION NOTES

ALL TEMPORARY TRAFFIC CONTROL DEVICES SHALL MEET ODOT'S "QUALITY STANDARDS FOR TEMPORARY TRAFFIC CONTROL DEVICES." CHANNELIZING DEVICES SHALL HAVE A MINIMUM HEIGHT OF 36 INCHES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF THE TEMPORARY TRAFFIC CONTROL DEVICES, AND SHALL BE RESPONSIBLE FOR REPAIRING OR REPLACING ANY DEVICE DURING CONSTRUCTION.

(TC-84) 60 CONSTRUCTION CALENDAR DAYS WERE USED TO COMPUTE THE SIGN DAY PAY ITEMS. THE AMOUNT OF CALENDAR DAYS USED TO COMPUTE THE SIGN DAY PAY ITEMS IS AN ESTIMATED QUANTITY ONLY, BASED ON THE CURRENT ODOT STANDARDS AND SUGGESTED CONSTRUCTION SEQUENCE FOR THIS PROJECT. THESE ESTIMATED SIGN DAY QUANTITIES MAY CHANGE AS THE PROJECT'S CONSTRUCTION TRAFFIC CONTROL IS MODIFIED DURING CONSTRUCTION.

EXISTING ROADWAY SHALL REMAIN OPEN DURING CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROPER BARRICADES, LIGHTS, SIGNING, AND DEVICES WITHIN THE LIMITS OF CONSTRUCTION. ALL CONSTRUCTION SIGNING WILL BE DONE ACCORDING TO STANDARDS SET FORTH IN THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION", AND AS SHOWN ON TCS STANDARD DRAWINGS.

(TC-85) THESE SIGNS MUST BE ON THE OKLAHOMA DEPARTMENT OF TRANSPORTATION LIST OF APPROVED CHANGEABLE MESSAGE SIGNS. FOR AN APPROVED LIST, GO TO THE QUALIFIED PRODUCT LIST WEBSITE AT <http://www.okladot.state.ok.us/traffic/qpl/index.php>.

THE CONTRACTOR SHALL BE CLOSE BY TO THE PROJECT TO MONITOR THE CONSTRUCTION TRAFFIC CONTROL DEVICES 24 HOURS A DAY, 7 DAYS A WEEK, AND SHALL IMMEDIATELY BE ON SITE TO RECTIFY ANY TRAFFIC CONTROL DEVICE THAT FAILS DURING CONSTRUCTION OR IS NOTIFIED BY THE ENGINEER. WORK SHALL BE DONE BY THE CONTRACTOR IN A MANNER APPROVED BY AND TO THE SATISFACTION OF THE ENGINEER.

SPECIAL TRAFFIC PAY QUANTITY NOTES

(SP-1) PORTABLE CHANGEABLE MESSAGE SIGN SHALL BE LOCATED WHERE DEEMED NECESSARY BY THE ENGINEER.

(SP-2) TYPE "C" WARNING LIGHTS ARE NOT REQUIRED.

TRAFFIC CONSTRUCTION PAY QUANTITY NOTES

(TC-26) CONSTRUCTION TRAFFIC CONTROL WILL BE IMPLEMENTED ACCORDING TO CONSTRUCTION PLANS, AND INSTALLED IN A MANNER APPROVED BY THE ENGINEER, IN ACCORDANCE WITH CHAPTER VI OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, (CURRENT EDITION), AND COMPLIANT WITH APPLICABLE O.D.O.T. STANDARD DRAWINGS. PRICE BID FOR THIS ITEM SHALL BE PAYMENT IN FULL FOR THE INSTALLATION, MAINTENANCE, AND SUBSEQUENT REMOVAL OF ALL NECESSARY CONSTRUCTION TRAFFIC CONTROL DEVICES REQUIRED FOR COMPLETION OF THE PROJECT.

ALL SIGNS AND BARRICADES WHICH ARE SHOWN WITH TYPE "A" LIGHTS IN THE STANDARD DRAWINGS SHALL HAVE THE CORRESPONDING LIGHT ATTACHED DURING NON-DAYLIGHT HOURS.

(TC-33) ALL CONSTRUCTION WORK ZONE SIGNS SHALL HAVE FLUORESCENT SHEETING. THE FLUORESCENT SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956 (LATEST REVISION).

THE MANUFACTURER SHALL FURNISH A TYPE "D" CERTIFICATION IN ACCORDANCE WITH O.D.O.T. STANDARD SPECIFICATIONS (CURRENT EDITION SUBSECTION 106.04). THE CERTIFICATION SHALL INCLUDE TEST RESULTS ON MATERIAL SUBMITTED FOR APPROVAL.

(TC-52) ANY USED TRUCK MOUNTED ATTENUATOR AND CHANGEABLE MESSAGE SIGN TO BE PLACED ON THIS PROJECT SHALL BE SUBJECT TO INSPECTION AND APPROVAL, BY THE OKLAHOMA DEPARTMENT OF TRANSPORTATION, TO ASSURE THAT THEY ARE IN GOOD WORKING CONDITION, PRIOR TO PLACEMENT ON THE PROJECT.

(TC-70) THIS ITEM IS AN ESTIMATED QUANTITY TO BE USED AS DEEMED NECESSARY BY THE ENGINEER

(TC-76) TRUCK MOUNTED ATTENUATORS USED ON THIS PROJECT SHALL HAVE PASSED ALL MANDATORY AND OPTIONAL TESTS LISTED IN NCHRP 350, TL-3 CRITERIA. THESE ITEMS ARE TO BE USED WHERE SHOWN IN THE STANDARD DRAWINGS OR AT THE DISCRETION OF THE ENGINEER ON SHADOW VEHICLES PROTECTING THE WORK AREAS AND TEMPORARY ROADSIDE HAZARDS.

(TC-77) THESE TRUCK MOUNTED ATTENUATORS (OR T.M.A.S) ARE TO BE INSTALLED ON NON-STATE OWNED TRUCKS HAVING A MINIMUM GROSS WEIGHT RATING OF 15,000 POUNDS. EACH OF THESE TRUCKS SHALL ALSO BE EQUIPPED WITH AN ARROW DISPLAY (TYPE B).

PAY QUANTITIES				
0300 TRAFFIC CONTROL				
ITEM	DESCRIPTION		UNIT	QUANTITY
876(A) 8482	(PL) TRUCK MOUNTED ATTENUATOR	(TC-52, 70, 76, 77)	SD	60.00
880(A) 8812	ARROW DISPLAY (TYPE C)	(TC-84)	SD	60.00
880(B) 8818	CONSTRUCTION SIGNS 0 TO 6.25 SF	(TC-26, 33, 84)	SD	480.00
880(B) 8821	CONSTRUCTION SIGNS 6.26 SF TO 15.99 SF	(TC-26, 33, 84)	SD	1020.00
880(B) 8824	CONSTRUCTION SIGNS 16.0 SF TO 32.99 SF	(TC-26, 33, 84)	SD	960.00
880(C) 8842	CONSTRUCTION BARRICADES (TYPE 111)	(TC-26, 84)	SD	360.00
880(C) 8848	WING BARRICADES	(TC-26, 84)	SD	240.00
880(E) 8860	WARNING LIGHTS (TYPE A)	(TC-26, 84)	SD	1,200.00
880(F) 8878	DRUMS	(SP-2) (TC-26, 84)	SD	1,560.00
880(G) 8890	CHANNELIZER CONES	(TC-26, 84)	SD	2,400.00
882(A) 8306	PORT. CHANGEABLE MESSAGE SIGN	(SP-1) (TC-52, 70, 85)	SD	60.00

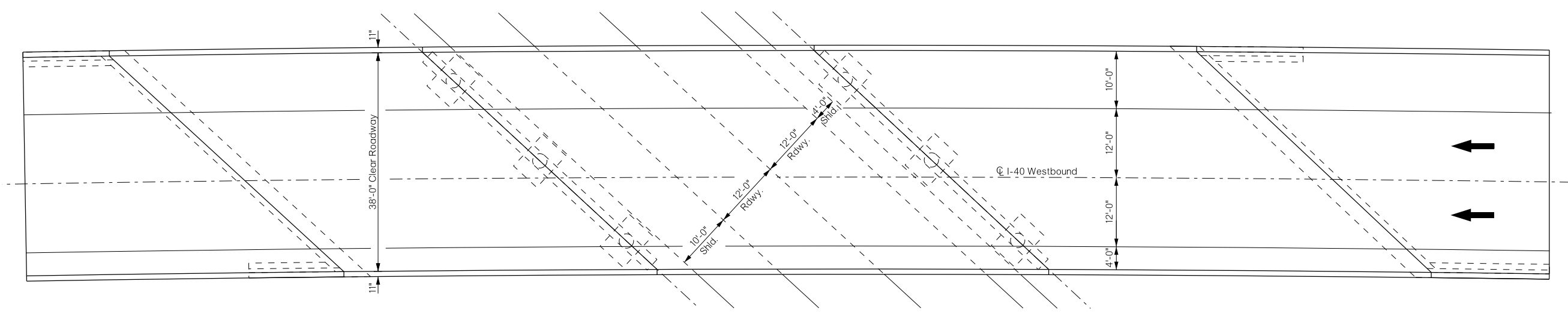
PREPARED BY:  
OKLAHOMA DEPARTMENT OF TRANSPORTATION  
TRAFFIC ENGINEERING DIVISION  
*Jami L. Short*  
DATE 6/20/2016

OKLA. REG. NO. 22542

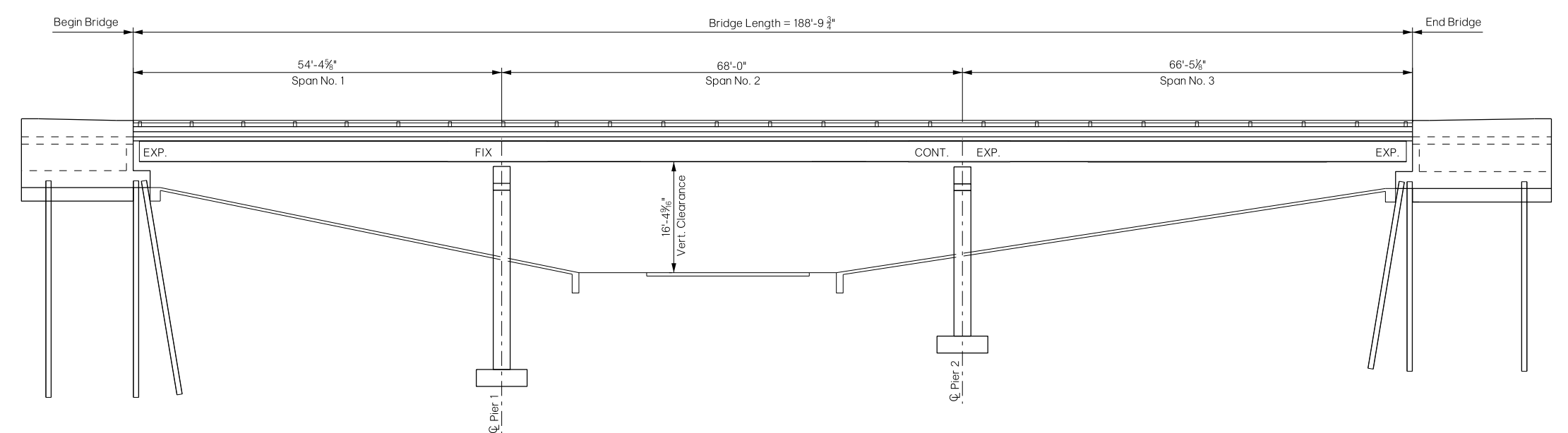
SUMMARY OF PAY QUANTITIES AND NOTES (TRAFFIC CONTROL)	Drawn	SRW	06/16
	Design	SEB	06/16
	Checked	JLS	06/16
			TRAFFIC ENGINEERING JAMI SHORT



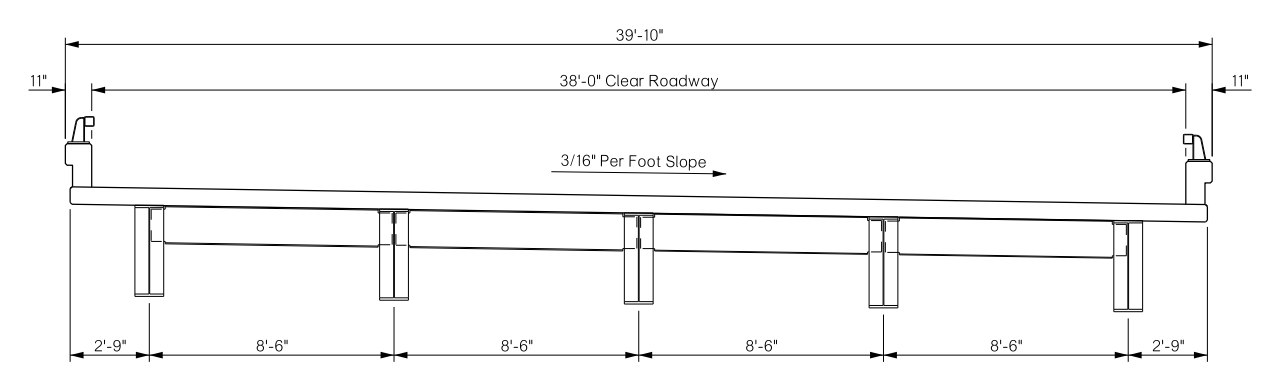
REVISIONS		
REV. NO.	DESCRIPTION	DATE



PLAN



ELEVATION



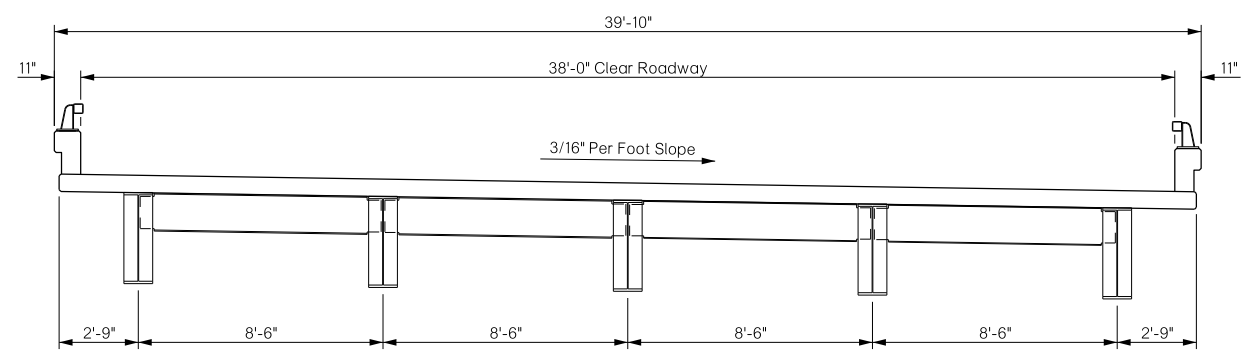
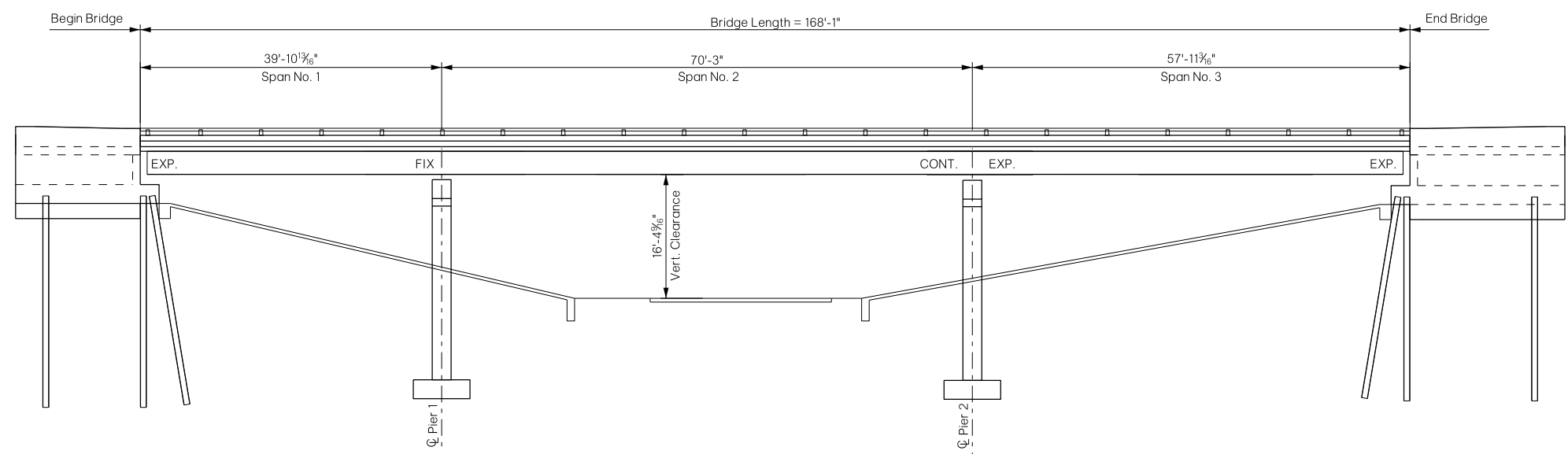
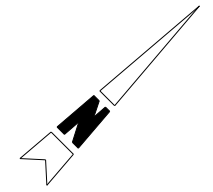
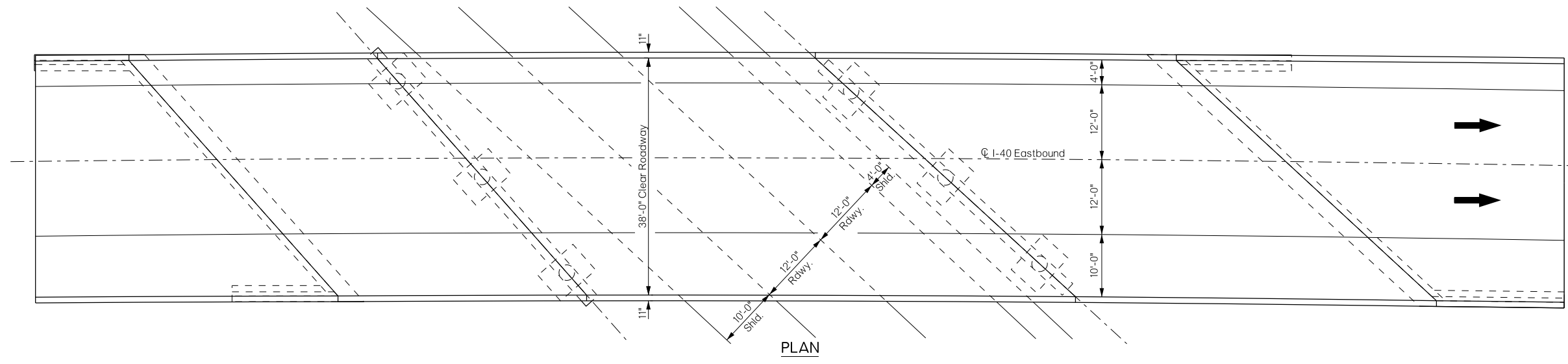
HALF END VIEW

HALF SECTION VIEW

TYPICAL SECTION

BRIDGE "A" I-40 W.B. OVER I-40 BUSINESS		CUSTER COUNTY		Design	N/A	N/A
GENERAL PLAN AND ELEVATION				Detail	WDY	02/16
				Check	DAH	03/16
Squad: HARJO				Engr: MOLLA-ESMAIL		
STATE OF OKLAHOMA		DEPARTMENT OF TRANSPORTATION		JOB/PIECE NO. 31700(04)		
				SHEET NO. B001		

REVISIONS		
REV. NO.	DESCRIPTION	DATE



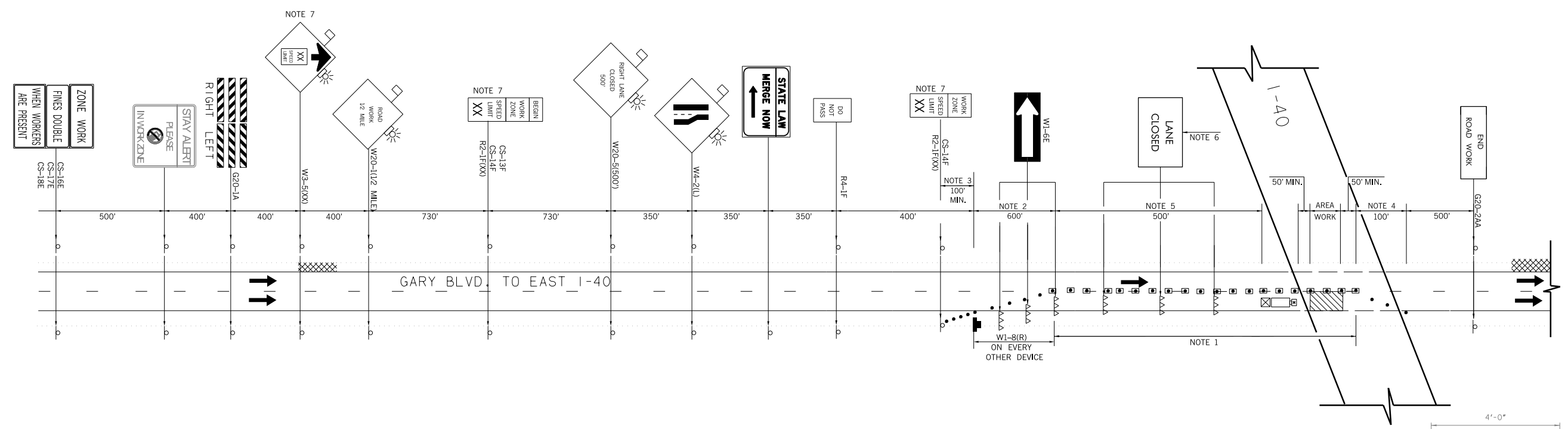
HALF END VIEW

HALF SECTION VIEW

TYPICAL SECTION

BRIDGE "B" I-40 E.B. OVER I-40 BUSINESS		CUSTER COUNTY		Design	N/A	N/A
GENERAL PLAN AND ELEVATION				Detail	WDY	02/16
				Check	DAH	03/16
Squad: HARJO				Engr: MOLLA-ESMAIL		
STATE OF OKLAHOMA		DEPARTMENT OF TRANSPORTATION		JOB/PIECE NO. 31700(04)		
				SHEET NO. B002		

REVISIONS		
REV. NO.	DESCRIPTION	DATE
1	Revised Incomplete JP #	6/20/2016



**NOTE 1**  
 MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES (FEET) SHALL BE TWICE THE POSTED SPEED LIMIT (M.P.H.) WITH THE FOLLOWING EXCEPTIONS, SPACING SHALL NOT EXCEED 50 FEET FOR CONES OR TUBE CHANNELIZERS. SPACING SHALL NOT EXCEED 75 FEET FOR CHANNELIZER CONES. SPACING SHALL NOT EXCEED 100 FEET FOR TYPE II BARRICADES, VERTICAL PANELS OR DRUMS.

**NOTE 2**  
 MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES (FEET) SHALL BE EQUAL TO THE POSTED SPEED LIMIT (M.P.H.) WITH THE FOLLOWING EXCEPTIONS, SPACING SHALL NOT EXCEED 25 FEET FOR CONES OR TUBE CHANNELIZERS. SPACING SHALL NOT EXCEED 50 FEET FOR TYPE II BARRICADES, VERTICAL PANELS OR DRUMS.

**NOTE 3**  
 A MINIMUM OF FIVE (5) CHANNELIZING DEVICES SHALL BE PLACED THROUGH THIS TAPER.

**NOTE 4**  
 DOWNSTREAM TAPERS SHALL CONTAIN A MINIMUM OF FOUR (4) CHANNELIZING DEVICES.

**NOTE 5**  
 A LONGITUDINAL BUFFER AREA, TO ALLOW WORKERS TIME TO EVACUATE THE WORK AREA, SHOULD BE PROVIDED. FOR GUIDELINES ON SETTING THE LENGTH OF THIS BUFFER, SEE STANDARD DRAWING TCS2-1-(LATEST REVISION). ACTUAL LENGTH SHALL BE DETERMINED BY FIELD CONDITIONS AND THE JUDGEMENT OF THE ENGINEER.

**NOTE 6**  
 TYPE III BARRICADES WITH SIGNS READING "LANE CLOSED" (R11-2) SHALL BE PLACED EVERY 2,000 FEET THROUGH ACTIVITY AREA. THESE TYPE III BARRICADES AND SIGNS MAY BE OMITTED ON MOVING OPERATIONS AND SHORT DURATION PROJECTS.

**NOTE 7**  
 CONSTRUCTION SPEED LIMIT TO BE DETERMINED BY THE DIVISION ENGINEER.

FOR ADDITIONAL INFORMATION ABOUT TAPER LENGTHS AND SPACING OF CHANNELIZING DEVICES, SEE STANDARD DRAWING TCS2-1-(LATEST REVISION).

OUTSIDE LANE CLOSURE IS THE REVERSE OF INSIDE. ADVANCE SIGNS SHALL REFLECT CORRECT LANE CLOSURE.

ALL DRIVES AND MEDIAN OPENINGS ARE TO REMAIN OPEN TO TRAFFIC.

- KEY:**
- o- SIGN
  - DRUM
  - ◻ CHANNELIZING CONE
  - ▨ WORK AREA
  - ➔ ARROW DISPLAY
  - ◻ SHADOW VEHICLE
  - ⊠ TRUCK MOUNTED ATTENUATOR
  - ▲▲▲ TYPE III BARRICADES

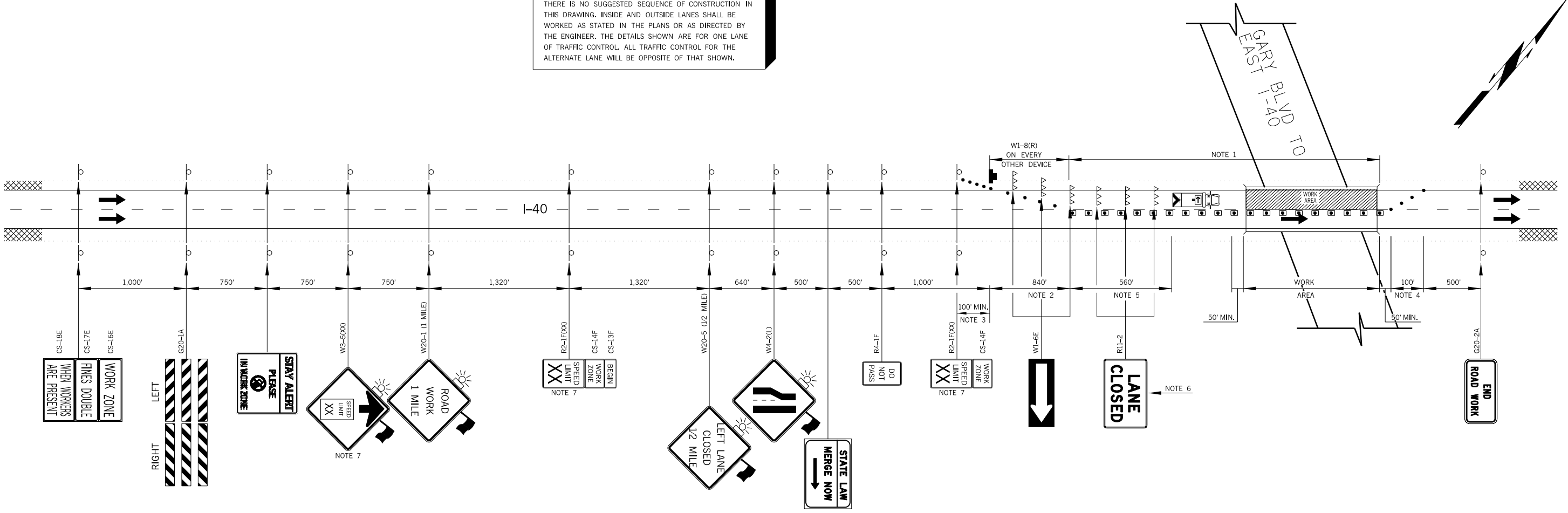


**COLOR:**  
 LEGEND, SYMBOL AND BORDER:  
 BLACK (NON-REFLECTORIZED)  
 RED (TRANSPARENT REFLECTORIZED)  
 BACKGROUND:  
 ▲ FLUORESCENT ORANGE (REFLECTORIZED)  
 \* FLUORESCENT YELLOW (REFLECTORIZED)  
 ● WHITE (REFLECTORIZED)

TRAFFIC CONTROL DETAIL I-40 BRIDGE OVER GARY BLVD			Drawn	SRW	06/16	
			Design	SEB	06/16	
STATE OF OKLAHOMA			Checked	JLS	06/16	
			TRAFFIC ENGINEERING JAMI SHORT			
DIVISION 5		JOB PIECE NO.	31700(04)		SHEET NO.	T001

DRAWING NOT TO SCALE

THERE IS NO SUGGESTED SEQUENCE OF CONSTRUCTION IN THIS DRAWING. INSIDE AND OUTSIDE LANES SHALL BE WORKED AS STATED IN THE PLANS OR AS DIRECTED BY THE ENGINEER. THE DETAILS SHOWN ARE FOR ONE LANE OF TRAFFIC CONTROL. ALL TRAFFIC CONTROL FOR THE ALTERNATE LANE WILL BE OPPOSITE OF THAT SHOWN.



**NOTE 1**  
 MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES (FEET) SHALL BE TWICE THE POSTED SPEED LIMIT (M.P.H.) WITH THE FOLLOWING EXCEPTIONS. SPACING SHALL NOT EXCEED 50 FEET FOR CONES OR TUBE CHANNELIZERS. SPACING SHALL NOT EXCEED 75 FEET FOR CHANNELIZER CONES. SPACING SHALL NOT EXCEED 100 FEET FOR TYPE II BARRICADES, VERTICAL PANELS OR DRUMS.

**NOTE 2**  
 MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES (FEET) SHALL BE EQUAL TO THE POSTED SPEED LIMIT (M.P.H.) WITH THE FOLLOWING EXCEPTIONS. SPACING SHALL NOT EXCEED 25 FEET FOR CONES OR TUBE CHANNELIZERS. SPACING SHALL NOT EXCEED 50 FEET FOR TYPE II BARRICADES, VERTICAL PANELS OR DRUMS.

**NOTE 3**  
 A MINIMUM OF FIVE (5) CHANNELIZING DEVICES SHALL BE PLACED THROUGH THIS TAPER.

**NOTE 4**  
 DOWNSTREAM TAPERS SHALL CONTAIN A MINIMUM OF FOUR (4) CHANNELIZING DEVICES.

**NOTE 5**  
 A LONGITUDINAL BUFFER AREA, TO ALLOW WORKERS TIME TO EVACUATE THE WORK AREA, SHOULD BE PROVIDED. FOR GUIDELINES ON SETTING THE LENGTH OF THIS BUFFER, SEE STANDARD DRAWING TCS2-1-(LATEST REVISION). ACTUAL LENGTH SHALL BE DETERMINED BY FIELD CONDITIONS AND THE JUDGEMENT OF THE ENGINEER.

**NOTE 6**  
 TYPE III BARRICADES WITH SIGNS READING "LANE CLOSED" (R11-2) SHALL BE PLACED EVERY 2,000 FEET THROUGH ACTIVITY AREA. THESE TYPE III BARRICADES AND SIGNS MAY BE OMITTED ON MOVING OPERATIONS AND SHORT DURATION PROJECTS.

**NOTE 7**  
 CONSTRUCTION SPEED LIMIT TO BE DETERMINED BY THE DIVISION ENGINEER.

FOR ADDITIONAL INFORMATION ABOUT TAPER LENGTHS AND SPACING OF CHANNELIZING DEVICES, SEE STANDARD DRAWING TCS2-1-(LATEST REVISION).



BORDER  
 R=1.5"  
 TH=0.75"  
 IN=0.75"

COLOR:  
 LEGEND, SYMBOL AND BORDER  
 BLACK (NON-REFLECTORIZED)  
 BACKGROUND:  
 ▲ FLUORESCENT ORANGE (REFLECTORIZED)  
 ● FLUORESCENT YELLOW (REFLECTORIZED)  
 ○ WHITE (REFLECTORIZED)  
 ■ RED (TRANSPARENT)

OUTSIDE LANE CLOSURE IS THE REVERSE OF INSIDE. ADVANCE SIGNS SHALL REFLECT CORRECT LANE CLOSURE.

SAME TRAFFIC CONTROL TO BE USED FOR BOTH EASTBOUND AND WESTBOUND CLOSURES.

- KEY:**
- SIGN
  - DRUM
  - ◻ CHANNELIZING CONE
  - ▨ WORK AREA
  - ➔ ARROW DISPLAY
  - 🚚 SHADOW VEHICLE
  - 🚚 TRUCK MOUNTED ATTENUATOR
  - ▲▲▲ TYPE III BARRICADES
- NOT TO SCALE

1-40 BRIDGE OVER GARY BLVD.			Drawn	SRW	6/16
			Design	SEB	6/16
STATE OF OKLAHOMA DEPARTMENT OF TRANSPORTATION			Checked	JLS	6/16
			TRAFFIC ENGINEERING JAMI SHORT		
DIVISION 5		JOB/PIECE NO. 31700(04)	SHEET NO. T002		